



APPENDIX 15-2

Traffic Management Plan

APPENDIX 15-2

CAHERMURPHY WEST WIND FARM

REVISION A – March 19th 2026

Alan Lipscombe Traffic & Transport Consultants Ltd
Claran, Headford, Co Galway

Email - Info@alipscombetraffic.ie
Tel – 093 34777
Mob – 087 9308134

Client: Cahermurphy Renewables DAC
September 19th, 2026
AL Project No: 11190

CONTENTS

1 INTRODUCTION

1.1 Purpose of note

2 DELIVERY OF ABNORMALLY SIZED LOADS TRANSPORTING TURBINE COMPONENTS

2.1 Proposed delivery route for abnormally sized loads

2.2 Traffic management measures for abnormally sized loads

3 DELIVERY ROUTES FOR GENERAL CONSTRUCTION TRAFFIC

4 PROPOSED WIND FARM SITE ACCESS JUNCTION

5 TRAFFIC MANAGEMENT MEASURE DURING CONSTRUCTION OF GRID CONNECTION ROUTE

6 GENERAL TRAFFIC MANAGEMENT MEASURES

APPENDICES

Appendix A Figures from the EIAR

Figure 15.1a Site location and turbine delivery route

Figure 15.1b Turbine delivery route autotrack assessment location plan

Figure 15.5a Proposed cable grid connection route

Figure 15.5b Proposed cable grid connection route – potential diversion routes

Figure 15.12 Location 7 – Proposed access junction on the L-6254, junction layout

Figure 15.13 Location 7 – Proposed access junction on the L-6254, junction layout, visibility splays

1 INTRODUCTION

1.1 Purpose of note

The purpose of this Traffic Management Plan (TMP) is to set out the traffic management measures that the Applicant will commit to provide during the construction stage of the proposed Cahermurphy Wind Farm (Proposed Project). The successful completion of the Proposed Project will require significant coordination and planning and a comprehensive set of mitigation measures will be put in place before and during the construction stage, in order to minimise the effects of the additional traffic generated on the surrounding road network. The measures are discussed under the following headings;

- Section 2 – Delivery of abnormally sized loads transporting turbine components.
- Section 3 – Delivery routes for general construction traffic.
- Section 4 – Proposed Wind Farm Site access junction.
- Section 5 – Traffic management measures during construction of the Proposed Grid Connection Route.
- Section 6 – General traffic management measures that will be implemented before, during and on completion of the construction of the Proposed Development.

It is confirmed that details for the TMP for the Proposed Project will be agreed with the Road Section of all relevant Local Authorities prior to construction.

All figures that are referenced in the EIAR prepared for the Proposed Project are included as Appended A of this TMP.

2 DELIVERY OF ABNORMALLY SIZED LOADS TRANSPORTING TURBINE COMPONENTS

2.1 Proposed delivery route for abnormally sized loads

The proposed port of entry for the large wind turbine components is the Port of Foynes in County Limerick. The proposed Turbine Delivery Route (TDR) from the port to the Proposed Wind Farm site is shown in Figure 15-1a. An assessment of the turning requirements of the abnormally large loads transporting the turbine components was undertaken at the various pinch points along the TDR, as identified in Figure 15-1b. The swept path assessment undertaken for these locations is discussed in Section 15.1.9 of the EIAR.

From the Port of Foynes the turbine delivery route is as follows;

- From the access road serving Foynes Port the route turns left (south) onto the N69 National Secondary Road at the existing priority junction.
- From this point the route heads east on the N69 for approximately 32kms, passing through various bends on the route.
- The route then turns left off the N69 at the Dock Road West Roundabout to head north onto the N18/M18 for approximately 30km to Junction 12 of the M18.
- At this point the route turns off the M18 onto the N85 National Road and heads west for approximately 3.8km passing through the Skehanagh Roundabout and the R458 Clareabbey Roundabout to the Rocky Road Roundabout where it then turns right to head northwest for approximately 260m on the N68 to the Kilrush Road Roundabout.
- From this point the route continues southeast on the N68 for approximately 23.1km passing through the village of Lissycasey before taking the right turn at the N68/R484 junction.
- From this point the route travels west along the R484 for approximately 4.7km to the village of Kilmihil, where the vehicles will perform a reverse manoeuvre through a temporary access road across two agricultural fields into the townland of Kilmihil, onto the L-2074 local road.
- The route then heads north on the L-2074 through Kilmihil before merging onto the L-2082.
- The route then travels north on the L-2082 for approximately 4km and passing through an agricultural field in the townland of Castlepark.
- The route then passes through an agricultural field north of Scoil Mhichíl in the townland of Cahermurphy, onto the L-2048.
- The route then heads east for approximately 290m on the L2048 before turning left onto the L-6254 heading north for approximately 1.2km before reaching the site entrance on the left.

The total length of the Turbine Delivery Route from Foynes Port to the access junction off the L-6254 is approximately 102 kms. All deliveries of abnormally sized loads will be made using Garda Síochána escorts and local transient traffic management measures put in place by the haulage company.

2.2 Traffic management measures for abnormally sized loads

The transportation of large components is challenging and can only be done following extensive route selection, route proofing and consultation with An Garda Síochána, the relevant local authorities and their road sections and roads authorities. Turbine components are usually transported in convoys of 3 vehicles at night when traffic is lightest. This will be undertaken in consultation with the road authorities, An Garda Síochána Traffic Corp and special permits are generally required.

A swept path analysis was undertaken at all potential pinch points using Autotrack in order to establish the locations where the wind turbine transporter vehicles will be accommodated, and the locations where some form of remedial measure may be required. While transient traffic management measures will be implemented by An Garda Síochána as each convoy travels along the delivery route, it is not anticipated that any sections of the local road network will be closed.

A dry run involving a vehicle adapted to replicate the geometry of the extended transport vehicles will be undertaken over the entire turbine delivery route prior to the delivery of turbine components.

3 DELIVERY ROUTES FOR GENERAL CONSTRUCTION TRAFFIC

The concrete and crushed stone required for the turbine foundations will be sourced from local, appropriately authorised quarries. All concrete deliveries will access the Proposed Wind Farm site via the new site access off the L-6254 located at the eastern end of the Proposed Wind Farm site, as shown in Figure 15-1a.

The closest operational quarry (Darragh Quarry) is located 25.5km southwest of the Site, with multiple quarries also located to the east and northeast of the site connected via national, regional and local roads. These deliveries will travel on the same haul route as for the abnormally sized loads.

All other wind turbine components delivered by standard HGVs will arrive at Foynes Port and will also be delivered via the same haul route as set out above.

4 PROPOSED WIND FARM SITE ACCESS JUNCTION

The location of the site access junction is shown in Figure 15-1a and is described below.

Site access for all vehicles on the L-6254

It is proposed to use the existing access junction on the L6254 that was constructed for the forestry and agricultural purposes to provide access for the Proposed Wind Farm. The existing junction layout together with the proposed road markings are shown in Figure 15-12. The junction design includes 13m junction radii and 1:10 tapers in accordance with TII Junction Design Guidelines (TII DN-GEO-03060) for junctions with HGV turning movements.

While there are no signs indicating the speed limit on the L-6254, based on the road width it is considered that a design speed of 60 km/h is appropriate for the local primary road. Based on this, in accordance with the County Clare Development Plan 2023 – 2029 Appendix 1 Development Management Guidelines (Table A2), visibility splays of 90m taken from a setback of 2.4m are required. These splays are shown in Figure 15-13.

5 TRAFFIC MANAGEMENT MEASURES DURING CONSTRUCTION OF PROPOSED GRID CONNECTION ROUTE

Traffic impacts and diversion routes identified for the Proposed Grid Connection Route works are included in Section 15.1.7.7 of the EIAR. Sections along the Proposed Grid Connection Route where there will be road closures and traffic diversions are identified.

It is proposed that the 110kV onsite substation is connected by 110kV underground cabling to the existing 110kV Moneypoint Substation located in the townland of Carrowdotia South, Co. Clare. The underground cabling route measures approximately 25km of which approximately 24.4km is located within the public road corridor with the remaining 0.6km located in private lands (0.1km), within the Proposed Wind Farm site (0.2km) or within the grounds of Moneypoint Substation (0.3km).

The extent of the Proposed Grid Connection that will impact on the public road network is considered in the 14 sections, as shown in Figure 15-5a, with 11 sections located on the public road network, 1 section off road in third party lands and 1 section off road in both the Proposed Wind Farm Site and Moneypoint Substation. The roads comprising each section of the route, together with the traffic management measure required during construction, together with the length and the estimated number of days required for construction, are set out in Table 15-29. Based on a construction rate of 100m per day, it is estimated that the Proposed Grid Connection will take approximately 250 working days to complete based on one construction crew operating at one location. In practice the construction duration may be significantly reduced using 2 construction crews operating at different locations on the route.

Table 15-29 Proposed Grid Connection underground cabling route link, traffic management measure, link length (km), construction duration (days)

Grid section	Proposed Grid Connection Section	Traffic management	Length (kms)	Construction duration (days)
Section 1	Off road at site	NA	0.2	2
Section 2	LS-6254, LS-2048	Closure	1.3	13
Section 3	Off road	NA	0.1	1
Section 4	LS-6186, LS-6194	Closure	3.3	33
Section 5	LS-6182, LP-2074, LP-2044	Closure	5.1	51
Section 6	LS-6118	Closure	1.8	18
Section 7	LS-6130, LP-6132	Closure	2.6	26
Section 8	LP-2050	Closure	1.9	19
Section 9	LP-2054	Closure	1.9	19

Grid section	Proposed Grid Connection Section	Traffic management	Length (kms)	Construction duration (days)
Section 10	LP-2054	Closure	2.4	24
Section 11	LP-2054	Closure	1.3	13
Section 12	LT-20543, LT-20544, LS-6154	Closure	2.5	25
Section 13	N67	Stop & Go	0.3	3
Section 14	Off road at Moneypoint	NA	0.3	3
Total			25.0	250

The on-road sections of the Proposed Grid Connection travels along 0.3km the N67 National Road, with the remaining 24.1km of the on-road route sections travelling along the local primary and secondary road network. An inspection of the route indicated that the majority of the Proposed Grid Connection will require a road closure at the point of construction on any given day during the construction phase. The exception to this is the short 0.3km section of the N67 where it is proposed that a Stop & Go traffic management system will be implemented in order to maintain 2-way traffic flow.

The potential diversion routes that may be used during the construction of the various sections of the Proposed Grid Connection are set out in Table 15-30 and shown in Figure 15-5b. For sections 2, 4, 5, 6, 7, 8, 9, 10, 11 and 12, which comprises 24.1 km of the total route, the diversions will result in low volumes of existing traffic from local roads being diverted onto other local roads, or onto roads of a higher standard, including the R473, R484, N67 and the N68.

It is also noted that crossings of the following roads will be required during the construction of the Proposed Grid Connection; LP-2082, R484, N68, R473 and LP-2058.

Table 15-30 Proposed Grid Connection underground cabling route link, link length (km), potential diversion route, length of diversion route (km), additional trip length (km)

Grid section	Proposed Grid Connection Section	Length (kms)	Potential diversion route	Length of diversion route (kms)	Additional trip length (kms)
Section 1	Off road at site	0.2	NA	NA	NA
Section 2	LS-6254, LS-2048	1.3	LS-6254, LS-6204, LP-2100, LP-2048	11.7	10.4
Section 3	Off road	0.1	NA	NA	NA
Section 4	LS-6186, LS-6194	3.3	LP-2082, R484	7.3	4
Section 5	LS-6182, LP-2074, LP-2044	5.1	R484, R483, LS-6118	12.5	7.4

Grid section	Proposed Grid Connection Section	Length (kms)	Potential diversion route	Length of diversion route (kms)	Additional trip length (kms)
Section 6	LS-6118	1.8	LS-6130, LS-6132, N68, LP-2044	9.4	7.6
Section 7	LS-6130, LP-6132	2.6	LS-6132, N68, LP-2044	8.9	6.3
Section 8	LP-2050	1.9	LS-6132, N68	6.1	4.2
Section 9	LP-2054	1.9	N68, LS-6160, LT-61601, R473	5.8	3.9
Section 10	LP-2054	2.4	R473, LP-2058	9.4	7
Section 11	LP-2054	1.3	LP-2058, LS-6304, N67	3.8	2.5
Section 12	LT-20543, LT-20544, LS-6154	2.5	N67	2.5	0
Section 13	N67	0.3	NA	NA	NA
Section 14	Off road at Moneypoint	0.3	NA	NA	NA
Total		25.0			

For the diversion routes shown in Figure 15-5b, the temporary additional trip length incurred by drivers during the construction of the Proposed Grid Connection will range from a minimum of 2.5km to a maximum of 10.4km. It should also be noted that the length of the diversion routes shown for the various sections of the Proposed Grid Connection are the longest that may be incurred, and are measured from either end of the section being constructed, and that in practice the number of trips that incur the full diversion will be very few. It is also noted that many drivers undertaking longer trips will divert onto other parallel routes further afield to avoid the closure, incurring shorter actual diversions.

It is estimated that the Proposed Grid Connection Route will take approximately 250 days, or approximately 12 months to construct.

6 GENERAL TRAFFIC MANAGEMENT MEASURES

A detailed TMP will be finalised and confirmatory detailed provisions in respect of traffic management agreed with the Roads Authorities and An Garda Síochána prior to construction works commencing on site. The detailed TMP will include the following:

Delivery of abnormal sized loads

- The delivery of turbine components is a specialist transport operation with the transportation of components carried out at night when traffic is at its lightest and the impact minimised.
- The deliveries will be made in consultation with the Local Authority and An Garda Síochána.
- It is estimated that 64 abnormal sized loads will be delivered to the site, comprising 22 convoys of 3 (1 convoy will have 1 vehicles only), undertaken over 22 separate nights.
- These nights will be spread out over an approximate period of 5 weeks and will be agreed in advance with the relevant authorities.
- For each convoy there will be two police escort vehicles that will stop traffic at the front and rear of the convoy of 3 vehicles in addition to two escort vehicles provided by the haulage company.

Other traffic management measures

- Traffic Management Coordinator – a competent Traffic Management Co-ordinator will be appointed for the duration of the project and this person will be the main point of contact for all matters relating to traffic management.
- Delivery Programme – a programme of deliveries will be submitted to the relevant County Councils (Clare and Limerick) in advance of deliveries of turbine components to site. Liaison with the Local Authorities and Transport Infrastructure Ireland (TII) will be carried out where required regarding requirements such as delivery timetabling. The programme will ensure that deliveries are scheduled in order to minimise the demand on the local network and minimise the pressure on the access to the site.
- Temporary traffic management measures during construction of Wind Farm Site at access junctions during construction – Temporary measures including signage at access junctions on the L6254.
- Temporary traffic management measures during construction of Grid Connection – Including signage and implementation of temporary traffic diversions.
- Information to locals – Locals in the area will be informed of any upcoming traffic related matters e.g. temporary lane/road closures (where required) or delivery of turbine components at night, via letter drops and posters in public places. Information will include the contact details of the Project Co-ordinator, who will be the main point of contact for all

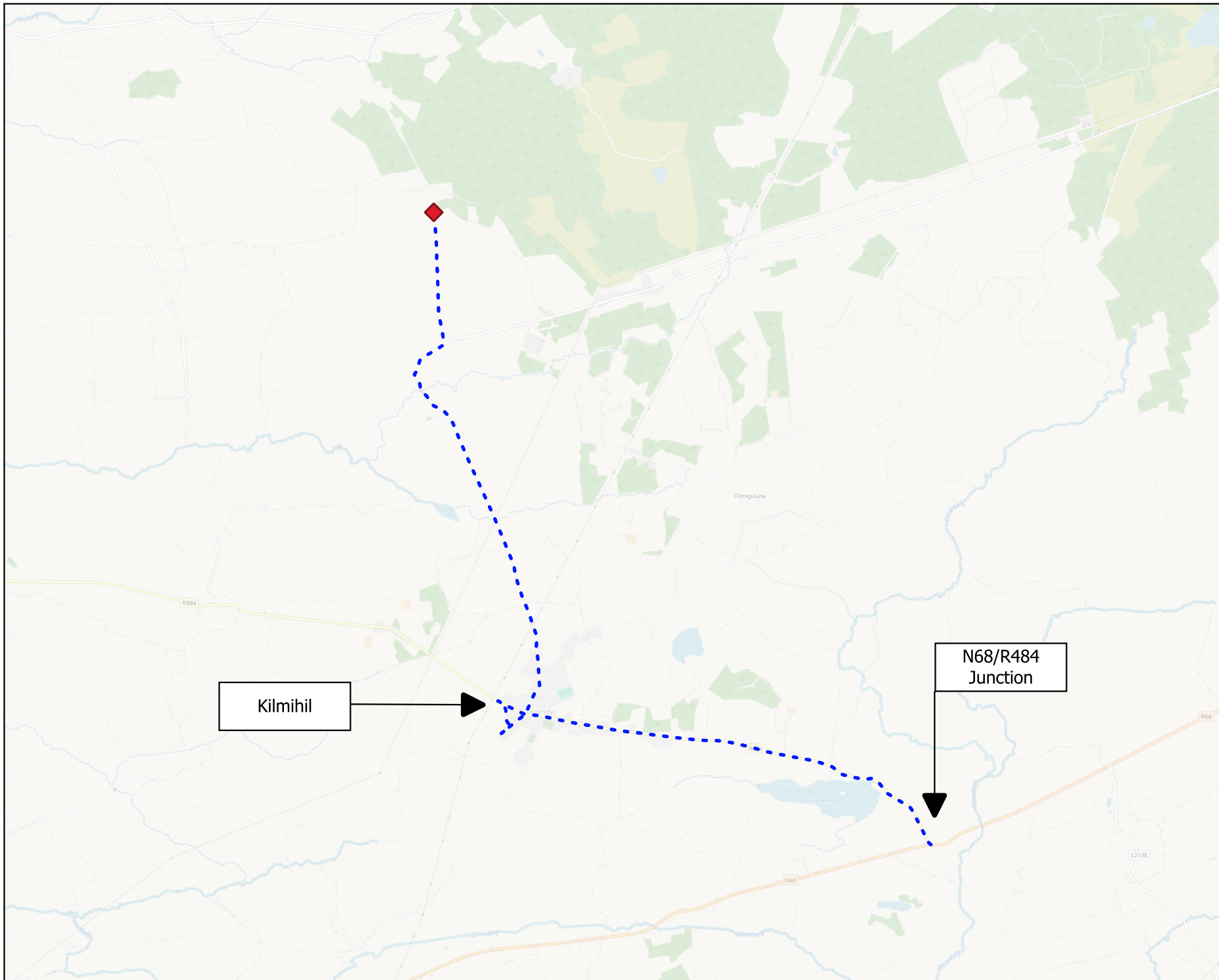
queries from the public or local authority during normal working hours. An "out of hours" emergency number will also be provided.

- A Pre and Post Construction Condition Survey – Where required by the Local Authorities, a pre-condition survey of roads associated with the Proposed Project will be carried out immediately prior to construction commencement to record an accurate condition of the road at the time. A post construction survey will be carried out after works are completed to ensure that any remediation works are carried out to a satisfactory standard. The timing of these surveys will be agreed with the local authority. All road surfaces and boundaries will be re-instated to pre-development condition, as agreed with the Local Authority Engineers.
- Liaison with the relevant local authority - Liaison with the County Councils and An Garda Síochána will be carried out during the delivery phase of the large turbine vehicles, when an escort for all convoys will be required. Once the surveys have been carried out and “prior to commencement” status of the relevant roads established, (in compliance with the provisions of the CEMP), the relevant Roads Sections will be informed of the names and contact numbers for the Project Developer/Contractor Site Manager as well as the Site Environmental Manager.
- Implementation of temporary alterations to road network at critical locations – at locations highlighted in Section 15.1.9.
- Identification of delivery routes – These routes will be agreed with the County Councils and adhered to by all contractors.
- Delivery times of large turbine components - The management plan will include the option to deliver the large wind turbine plant components at night in order to minimise disruption to general traffic during the construction stage.
- Travel plan for construction workers – While the assessment above has assumed the worst case in that construction workers will drive to the site, the Contractor will be required to provide a travel plan for construction staff, which will include the identification of routes to / from the site.
- Additional measures - Various additional measures will be put in place in order to minimise the effects of the development traffic on the surrounding road network including wheel washing facilities on site and sweeping / cleaning of local roads as required.
- Re-instatement works - All road surfaces and boundaries will be re-instated to pre-development condition, as agreed with the local authority engineers.

It is confirmed that details for the Traffic Management Plan for the Proposed Project will be agreed with the Road Section of Clare County Council prior to construction and contact will be maintained with the Road and Traffic Section throughout the construction phase.

APPENDIX A FIGURES FROM THE EIAR

- Figure 15.1a Site location and turbine delivery route
- Figure 15.1b Turbine delivery route autotrack assessment location plan
- Figure 15.5a Proposed cable grid connection route
- Figure 15.5b Proposed cable grid connection route – potential diversion routes
- Figure 15.12 Location 7 – Proposed access junction on the L-6254, junction layout
- Figure 15.13 Location 7 – Proposed access junction on the L-6254, junction layout, visibility splays



Map Legend

- - - Proposed Turbine Delivery Route
- ◆ Proposed Site Access Junction

Kilmihil

N68/R484
Junction



Microsoft product screen shots reprinted
 with permission from Microsoft Corporation
 © Ordnance Survey Ireland. All rights reserved.
 Licence number CYAL50267517

Drawing Title
**Site Location and Turbine
 Delivery Route**

Project Title
**Cahermurphy West Wind
 Farm**

Drawn By MC	Checked By EMC
-----------------------	--------------------------

Project No. 230843	Drawing No. Figure 15-1a
------------------------------	------------------------------------

Scale 1:50,000	Date 19.03.2026
--------------------------	---------------------------

MKO
 Planning and
 Environmental
 Consultants

Tuam Road, Galway
 Ireland, H91 VV84
 +353 (0) 91 735611
 email: info@mkofireland.ie
 Website: ww.mkofireland.ie



Map Legend

- - - Proposed Turbine Delivery Route
- ◆ Autotrack Assessment Locations

Site Access Junction

Location 7

Location 5

Location 6

Location 4

Location 3

Kilmihil

Location 2

N68/R484 Junction

Location 1



Microsoft product screen shots reprinted with permission from Microsoft Corporation © Ordnance Survey Ireland. All rights reserved. Licence number CYAL50267517

Drawing Title
Turbine Delivery Route Autotrack Assessment Location Plan

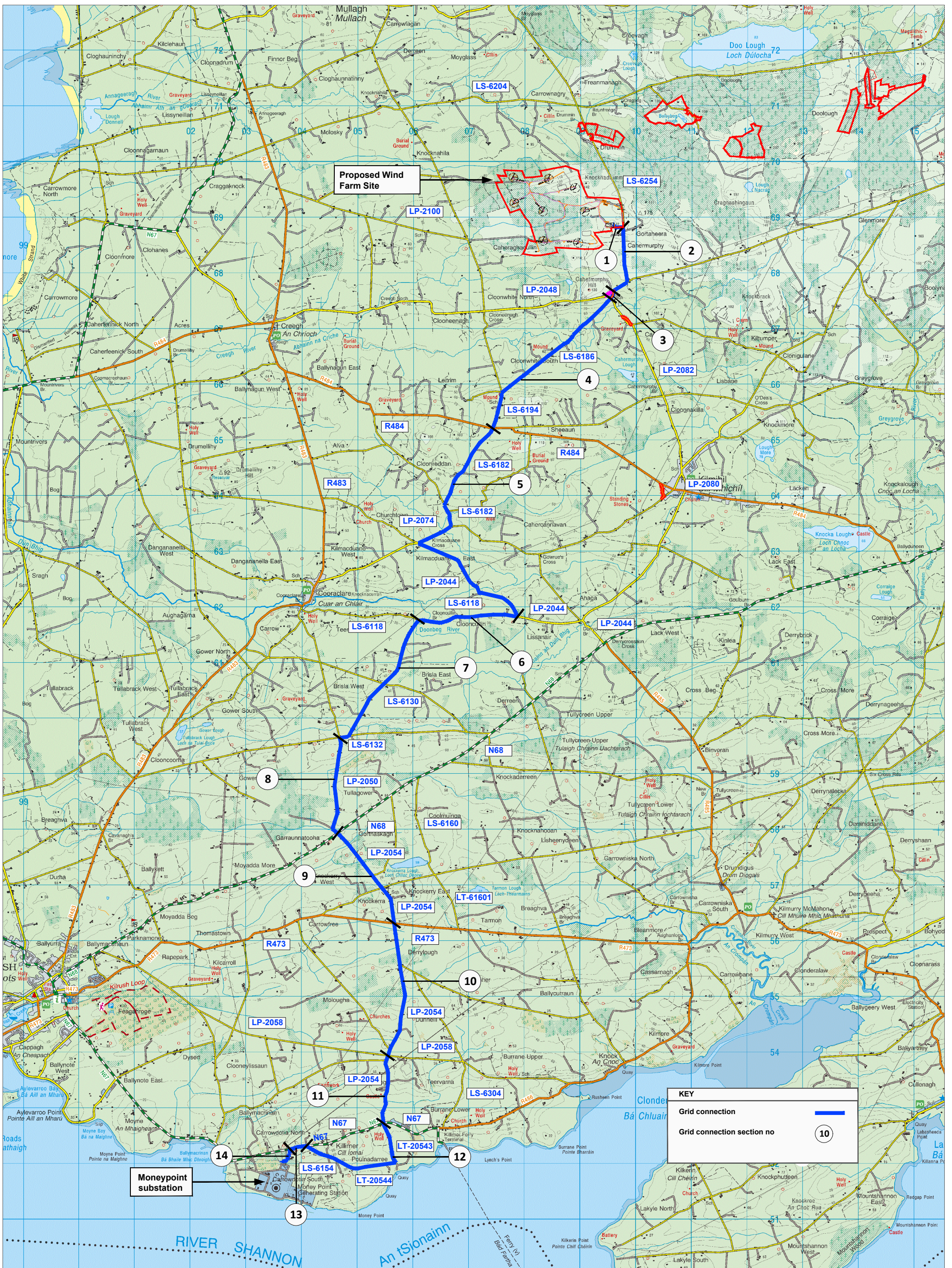
Project Title
Cahermurphy West Wind Farm

Drawn By MC	Checked By EMC
-----------------------	--------------------------

Project No. 230843	Drawing No. Figure 15-1b
------------------------------	------------------------------------

Scale 1:50,000	Date 19.03.2026
--------------------------	---------------------------

MKO
 Planning and Environmental Consultants
 Tuam Road, Galway
 Ireland, H91 VV84
 +353 (0) 91 735611
 email: info@mkofireland.ie
 Website: ww.mkofireland.ie



NOTES:

PLANNING DRAWING ONLY - NOT FOR CONSTRUCTION PURPOSES

Figure 15-5a Proposed Grid Connection Route

PROJECT: Cahermurphy West Wind Farm, Co Clare

CLIENT: Cahermurphy Renewables DAC

PROJECT NO: 11190

DATE: 22.01.26

SCALE: NTS

DRAWN BY: AL

ALAN LIPSCOMBE
TRAFFIC & TRANSPORT CONSULTANTS

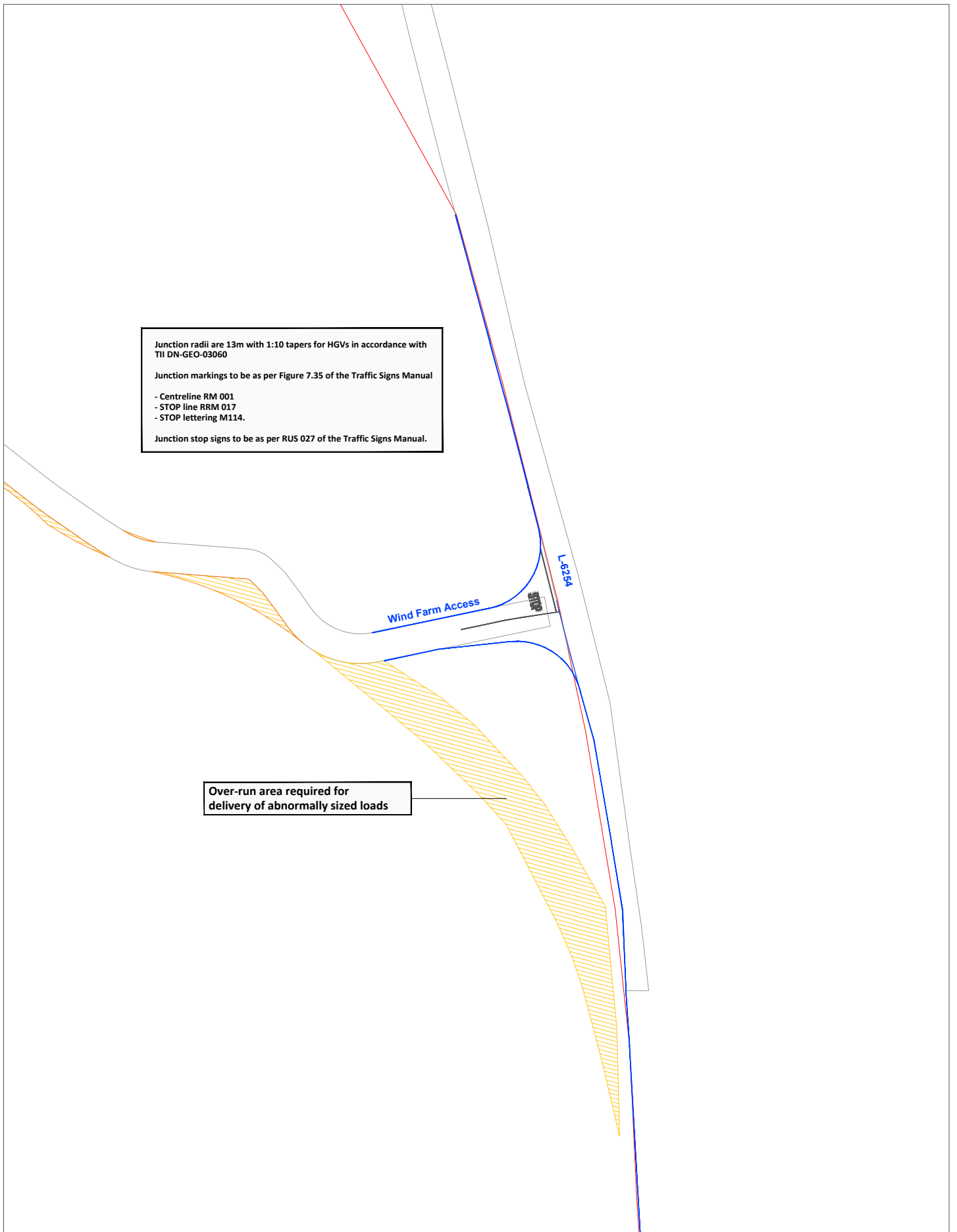


Figure 15-12 Location 7 - Proposed access junction on the L-6254, junction layout

PROJECT: Cahermurphy West Wind Farm, Co Clare		
CLIENT: Cahermurphy Renewables DAC	SCALE: 1:1000	
PROJECT NO: 11190	DATE: 16.01.26	DRAWN BY: AL

ALAN LIPSCOMBE
TRAFFIC & TRANSPORT CONSULTANTS

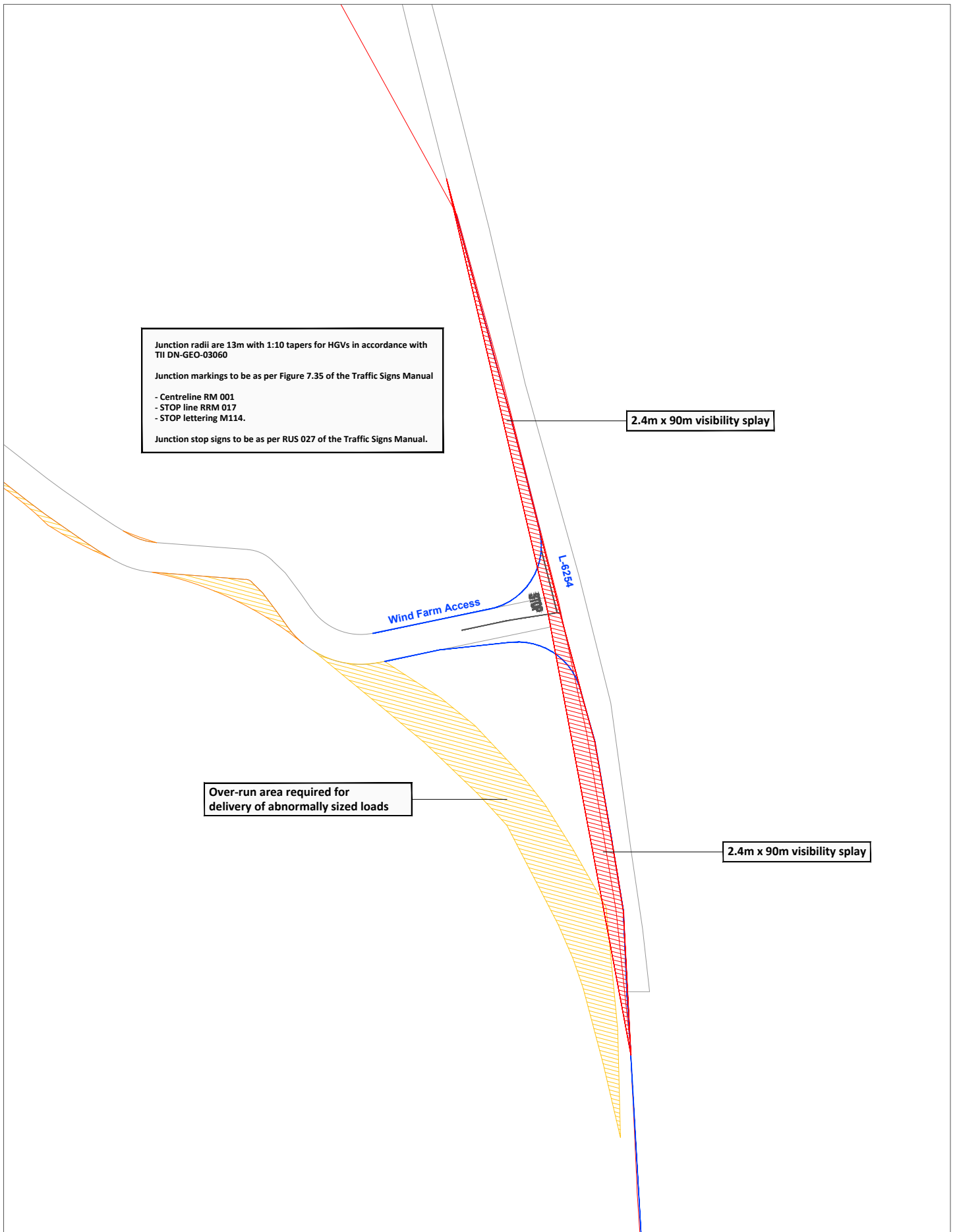


Figure 15-13 Location 7 - Proposed access junction on the L-6254, junction layout, visibility splays

PROJECT: Cahermurphy West Wind Farm, Co Clare		
CLIENT: Cahermurphy Renewables DAC	SCALE: 1:1000	
PROJECT NO: 11190	DATE: 16.01.26	DRAWN BY: AL

ALAN LIPSCOMBE
TRAFFIC & TRANSPORT CONSULTANTS