



APPENDIX 15-4

ROAD SAFETY AUDIT

Cahermurphy West Wind Farm, Co. Clare

Stage 1 Road Safety Audit

Alan Lipscombe Traffic & Transportation Consultants

March 2026

Cahermurphy West Wind Farm, Co. Clare

Stage 1 Road Safety Audit

March 2026

Notice

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Document History

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1. Introduction

1.1 Report Context

This report describes the findings of a Stage 1 Road Safety Audit associated with the Cahermurphy West Wind Farm, Co. Clare.

The Audit has been completed by Traffico on behalf of Alan Lipscombe Traffic & Transportation Consultants.

1.2 Details of Site Inspection

Date	Daylight / Darkness	Weather & Road Conditions
13 March 2026	Daylight	Overcast with wet road pavements.

Table 1.1 – Site Inspection Details

1.3 The Road Safety Audit Team

The members of the Road Safety Audit Team have been listed following:

Status	Name / Qualifications	TII Auditor Reference No:
Audit Team Leader (ATL)	Martin Deegan BEng(Hons) MSc CEng FIEI	MD101312
Audit Team Member (ATM)	Shane Kearns MEng BEng (Hons), RSACert, MIEI, MTPS	SK*364

Table 1.2 – Audit Team Details

1.4 Design Information Examined as Part of the Audit Process

The following design information was examined as part of the Road Safety Audit (RSA) process:

Drawing No.	Drawing Title	Revision
Figure 15-14	Location 8 – Proposed Access Junction on the L-6254, Junction Layout, Visibility Splays	-
Figure 15-15	Location 8 – Proposed Access Junction on the L-6254, Autotrack Assessment – Blade Transporter	-

Table 1.3 – Designers Drawing List

1.5 Road Safety Audit Compliance

Procedure and Scope

This Road Safety Audit has been carried out in accordance with the procedures and scope set out in TII publication number GE-STY-01024 - Road Safety Audit.

As part of the road safety audit process, the Audit Team have examined only those issues within the design which relate directly to road safety.

Compliance with Design Standards

The road safety audit process is not a design check, therefore verification or compliance with design standards has not formed part of the audit process.

Minimizing Risk of Collision Occurrence

All problems described in this report are considered by the Audit Team to require action in order to improve the safety of the scheme and minimise the risk of collision occurrence.

2. Road Safety Issues Identified

2.1 Problem: Visibility Splays Partially Obstructed by Vegetation

Location: Boundary Treatment Either Side of Access Road Junction Onto L-6254

Existing vegetation on both sides of the access junction may obstruct visibility, making it difficult for drivers joining the L-6254 to see approaching traffic and increasing the risk of rear-end or side-impact collisions.

Figure 2.1 – Existing Vegetation In Ditches Either Side of access road junction



Recommendation

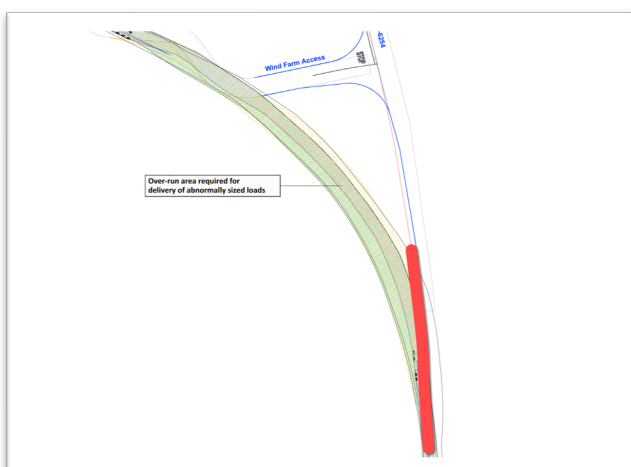
The Designer should ensure that sufficient visibility can be achieved on both sides of the access road.

2.2 Problem: Passing Motorists Accessing the Via Overrun Area

Location: Overrun Area to South of L-6254 Access Road Junction

The over-run area might create confusion for passing motorists, who could access the overrun area by mistake, leading to sudden braking, loss of control or conflicts with construction traffic.

Figure 2.2 – Location Where Passing Motorists Could Access the Overrun Area (When Not in Use)



Recommendation

The Designer should ensure that a suitable barrier is put in place to prevent passing motorists from accessing the abnormal load overrun area when it is not in use.

3. Audit Team Statement

3.1 Certification & Purpose

We certify that we have examined the drawing(s) listed in Chapter 1 of this Report.

Sole Purpose of the Road Safety Audit

The Road Safety Audit has been carried out with the sole purpose of identifying any features of the design which could be removed or modified to improve the road safety aspects of the scheme.

3.2 Implementation of RSA Recommendations

The problems identified herein have been noted in the Report together with their associated recommendations for road safety improvements.

We (the Audit Team) propose that these recommendations should be studied with a view to implementation.

Audit Team’s Independence to the Design Process

No member of the Audit Team has been otherwise involved with the design of the measures audited.

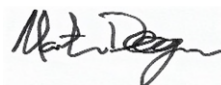
3.3 Road Safety Audit Team Sign-Off

Martin Deegan

Audit Team Leader
Road Safety Engineering Team

traffico

Signed:



Date:

16th March 2026

Shane Kearns

Audit Team Member
Road Safety Engineering Team

traffico

Signed:



Date:

16th March 2026

4. Designers Response

4.1 Using the Feedback form to Respond to the Road Safety Audit

The Designer and the Client should prepare an Audit Response for each of the recommendations using the Road Safety Audit Feedback Form attached in Appendix A.

When completed, this form should be signed by the Designer and the Client and returned to the Audit Team for consideration. See flow-chart following for further description.



Figure 4.1 – Road Safety Audit Sign-Off and Completion Process

4.2 Returning the Completed Feedback Form

The Designer should return the completed Road Safety Audit Feedback Form attached in Appendix A of this report to the following email address:

- Email address: martin@traffico.ie
- Telephone: 01 699 1551

The Audit Team will consider the Designer’s response and reply indicating acceptance or otherwise of the Designers response to each recommendation.

4.3 Triggering the Need for an Exception Report

If the Designer and Audit Team cannot agree on how to address a safety issue identified as part of the road safety audit process, then the Designer must prepare an Exception Report for each disputed item in the audit report.

Refer to *GE-STY-01027 Road Safety Audit Guidelines, Section 3.4.3 Exception Report* for further guidance.

Appendix A

A.1 Road Safety Audit Feedback Form

Road Safety Audit Feedback Form

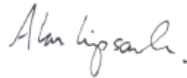


Scheme: Cahermurphy West Wind Farm, Co. Clare

Audit Stage: Stage 1 Road Safety Audit

Audit Date: 16 March 2026

Problem Reference (Section 2)	Designer Response Section			Audit Team Response Section
	Problem Accepted (yes / no)	Recommended Measure Accepted (yes / no)	Alternative Measures or Comments	Alternative Measures Accepted (yes / no)
2.1	Yes	Yes	The vegetation either side of the access junction will be removed as recommended.	Comment noted and accepted.
2.2	Yes	Yes	The over-run area for abnormally sized loads will be closed as a default and opened only on the nights that the abnormally sized loads are delivered, which will be accompanied with a Garda escort.	Comment noted and accepted.

**The Designer should complete the Designer Response Section above, then fill out the designer details below and return the completed form to the Road Safety Audit Team for consideration and signing.*

Designer's Name:	Alan Lipscombe	Designer's Signature:		Date:	18 th March 2026
Audit Team's Name:	Martin	Audit Team's Signature:		Date:	20 th March 2026
Client's Name:	David Heelan	Client's Signature:		Date:	18 th March 2026



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